



# Meeting Summary

## **Nantucket Memorial Airport Master Plan**

Working Group meeting, December 18, 2014, 3:00 p.m. • ARFF Building, Nantucket Airport  
Attendees: See page 3.

### **Welcome and Introductions**

Anne McKinnon, Jacobs Engineering Group, led introductions. Working Group meetings are open to the public and anyone is welcome to attend.

### **Review of Master Plan Status**

Bill Richardson, Jacobs, gave an update on Master Plan progress since the May Working Group workshop. Two parallel efforts have been underway: identifying projects to include in the Capital Improvement Program (CIP) and the long-range policy issues that have emerged as important through the Master Plan process. The next steps in the planning effort are to finalize the five-year CIP improvements plan; develop the Financial Business Plan; prepare the Airport Layout Plan for FAA approval; and prepare the Environmental Notification Form for the five-year improvements plan. The Master Plan should be complete in spring 2015.

### **Master Plan Improvement Concepts**

Richardson described the five categories of improvement concepts that are key to the CIP:

1. Safety and Security;
2. Capacity;
3. Efficiency;
4. Revenue Enhancement; and
5. Environmental/Sustainability.

#### ***I. Recommended Safety and Security Concepts:***

1. Terminal apron repaving in seven phases
3. Create new jet airline parking position to avoid Runway 15 protected airspace
4. Redesign South Apron in five phases to enhance safety, reduce congestion and meet FAA wingspan standards
5. Separate taxiways E and G for compliance and to increase operational efficiency
6. Create Runway 24 high-speed exit taxiway for safety, noise reduction and operational efficiency
7. Runway 33 high-speed exit taxiway and stub taxiway to Runway 33 end.
8. Information Technology and security system upgrades
  - a. Integrate terminal P.A. system with Flight Information Display
  - b. Vehicular and pedestrian access controls
  - c. Consolidate communications systems in main terminal
  - d. New communications pathways

#### ***II. Terminal and Airfield Capacity Concepts***

1. Temporary tent/secure hold room to address seasonal congestion
3. Terminal 10-year plan to convert bag claim to second hold room and build new bag claim
4. Terminal 20-year plan for major terminal renovation/expansion to add TSA, concession, Airport Security office space and restrooms.



### ***III. Efficiency Concepts***

1. New FAA request for ACK to fund balance of Tower rehab (\$1 million)
2. New Airport Ground Support Equipment (GSE) storage building
3. New 10,000 s.f. addition to Snow Removal Equipment garage to consolidate GSE
4. Manager's House/Thompson House rehab

Tom Rafter, ACK Manager, gave an update on the Tower rehab. The Airport received a \$3.4 million earmark from FAA. At 75% design, the construction estimate was \$1 million more than the earmark. The Airport reduced the scope of the rehab to cut costs, but there is still \$1 million needed to complete the rehab. The additional \$1 million would be paid by the Airport to complete phase 2 of the basic improvements. Rafter said construction bids are due in February and there's a Sept. 30 deadline for the project. There was discussion about the manager's house recommendation. The issue of housing for the Airport manager has been around for 10 years. It was agreed that the recommendation to provide housing for the Airport manager should be independent of action involving the existing building or proposed structure.

### ***IV. Revenue Enhancement Concepts***

1. Swap or lease non-aviation surplus parcels
2. Expand Bunker Area Industrial Development to enhance Airport revenue and address island-wide demand (downtown bulk fuel farm will be relocated to Town-owned land east of the ACK Bunker Area)
3. Develop a remote commercial vehicle parking area on Bunker Road to address existing demand
4. Lease four Delta non-aviation parcels not including ballfields (parcels on Sun Island Road, Nobadeer Farm Road, and Nobadeer Farm Road/Milestone Road); rezoning needed to permit housing
5. Encourage private-sector development of large GA jet hangars
6. Encourage private-sector development of GA hangar-commercial combo space to address demand
7. Encourage private-sector development of GA on North Apron to address demand
8. Review rates and charges with the goal of establishing an equitable system (includes ground leases, parking, wing-span vs. weight-based fees, etc.) and benchmarking comparable airports' rates and charges.

Discussion expanded on and clarified the proposed concepts. Tom Quigley, Surfside Assn., suggested weight-based charges with a surcharge for over-sized wings. Peter Farrell, Cape Air, asked about demand for hangars. Rafter said there is demand for small, cheap hangar space, and the Airport may want to be able to respond to requests for big hangars. On the issue of housing, Rafter said if the Airport were to participate in a hotel development it would require a block of rooms for workers on the island. Review of parking lot operations and rates could include restructuring the overnight rate, new equipment, private operator, etc.

### ***V. Environmental and Sustainability Concepts***

1. Develop 17-acre solar array in Bunker Area
2. Develop 23-acre solar array adjacent to Runway 24
3. Promote increased use of bus, ride share and bike access to ACK
4. Reduce ramp lighting with Pilot-Controlled Lighting (PCL)
5. Coastal management
  - a. Control access to bank?
  - b. Protect bank from erosion to reduce risk to airfield?

Arthur Gasbarro, Airport Commission, suggested dropping the solar array concept at Runway 24 due to rare species environmental mitigation costs. There was general agreement that controlling access to the coastal bank is needed, but the concept of actively protecting the bank from erosion was seen by many to be unnecessary at this time. Ernie Steinauer, Mass. Audubon, said keep monitoring the rate of erosion and reevaluate later. Ramp



electrification to reduce APU use was discussed, but many felt this is a low priority since the Airport provides portable APUs.

### **Q & A/Discussion**

The group reviewed some of the concepts and answered priority preference questions using the Keypoint touchpads related to the five improvement concepts categories. See attached for questions and results. The results show the Working Group prioritized many of the proposed projects as “Year 1” activities. Airport staff will consider the Working Group input as it works to balance the Airport’s needs.

### **Next Steps**

- Finalize the 5-year CIP improvements plan
- Develop the Financial Business Plan
- Prepare the ALP for FAA approval
- Prepare Environmental Notification Form for the 5-year improvements plan

No additional Working Group meetings are planned. Updates and final master plan elements will be sent electronically to Working Group members and posted on the Nantucket Airport Master Plan webpage. Master Plan information will be disseminated to the public in spring 2015 using a variety of techniques.

The meeting adjourned at 5:04 p.m. If your recollection of the meeting is different, please let Mae Williams know as soon as possible ([mwilliams@nantucketairport.com](mailto:mwilliams@nantucketairport.com)).

## **Nantucket Airport Master Plan Working Group meeting attendees**

December 18, 2014

<b>Name</b>		<b>Organization</b>
Diane	<b>Archer</b>	resident*
Dan	Drake	Chair, Nantucket Airport Commission
Bob	<b>Egan</b>	Nantucket Flying Association
Peter	Farrell	Cape Air
Arthur	<b>Gasbarro</b>	Vice chair, Nantucket Airport Commission
Katie	Hogue	Jacobs Engineering
P. J.	<b>Martin Smith</b>	Nantucket Chamber of Commerce* (first half of meeting)
Anne	McKinnon	Jacobs Engineering
Pat	Newton	Surfside Association
Andrea	Planzer	Nantucket Airport Commission
Tom	<b>Quigley</b>	Surfside Association
Tom	<b>Rafter</b>	Manager, Nantucket Airport
Allen	<b>Reinhard</b>	Nantucket Civic League
Bill	Richardson	Jacobs Engineering
Ernie	<b>Steinauer</b>	Mass. Audubon Society
Jeanette	Topham	Nantucket Airport Commission
Mae	Williams	Nantucket Airport

Working Group Members Absent:

Sam Bailey, Armando Cardenas, Carol Dunton, Leslie Johnson, David Sylvia, Andrew Vorce

Working Group members shown in **bold**

\* Participated by telephone and WebEx

# **ACK Working Group Priorities**

Meeting #4 • December 18, 2014

Discussion Questions



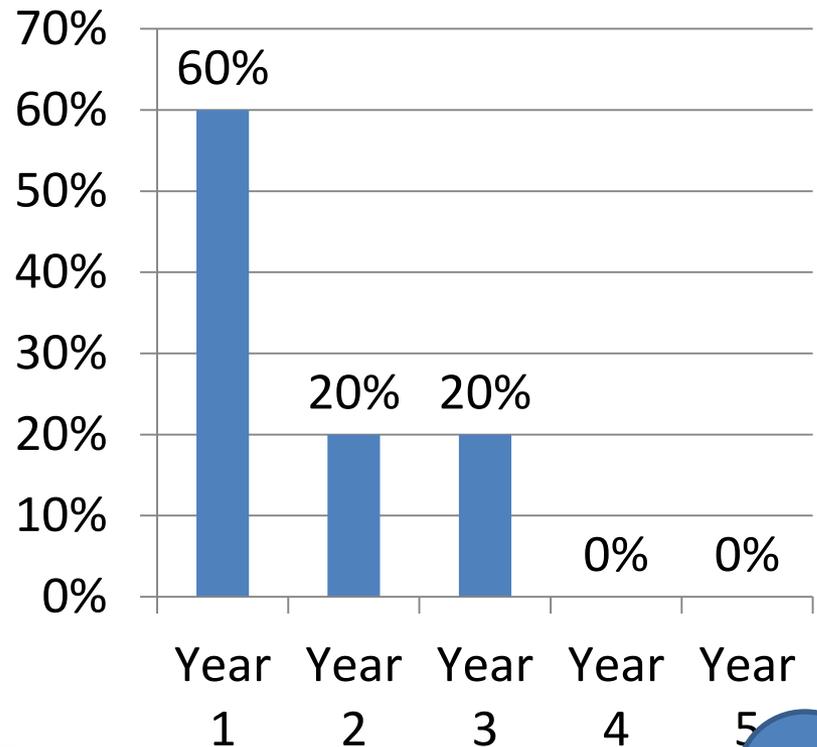
# Proposed Projects Priorities



# Terminal Secure Hold Room Options

## Select Timeframe Priority (year)

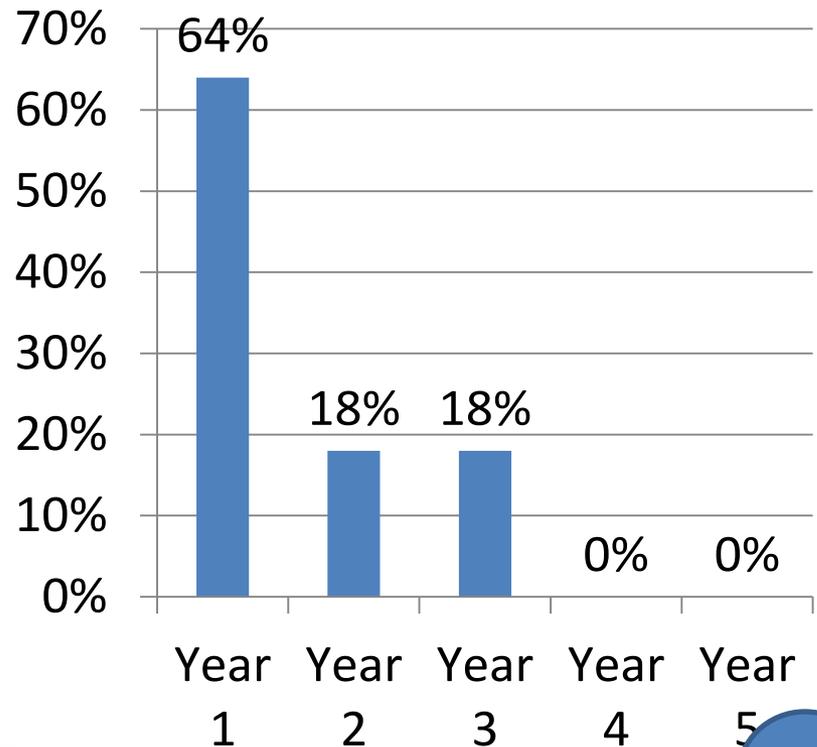
1. Year 1
2. Year 2
3. Year 3
4. Year 4
5. Year 5



# Airport Manager's/Thompson House

## Select Timeframe Priority (year)

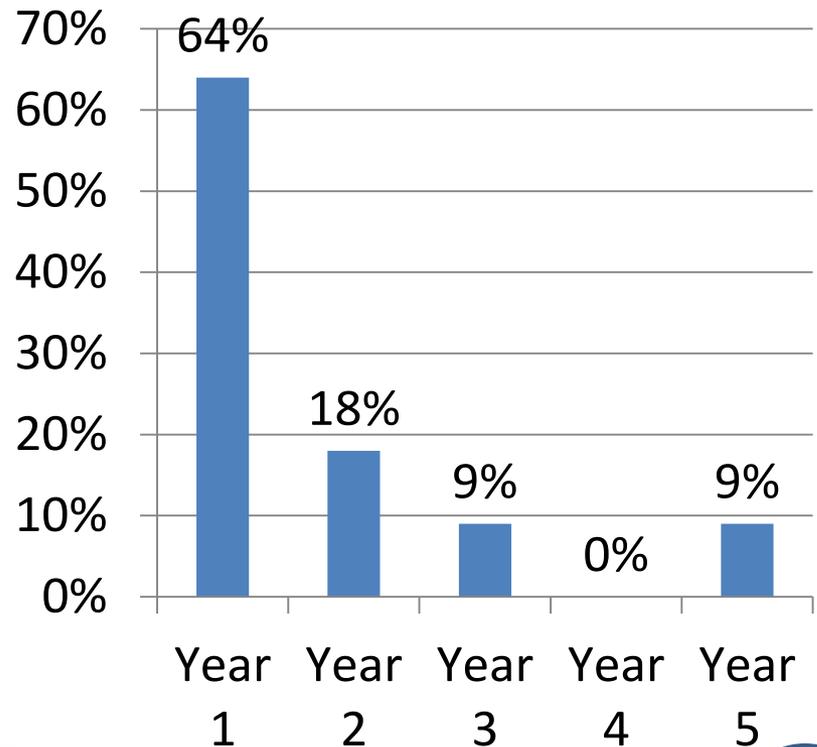
1. Year 1
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3. Year 3
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5. Year 5



# Air Traffic Control Tower Rehabilitation

## Select Timeframe Priority (year)

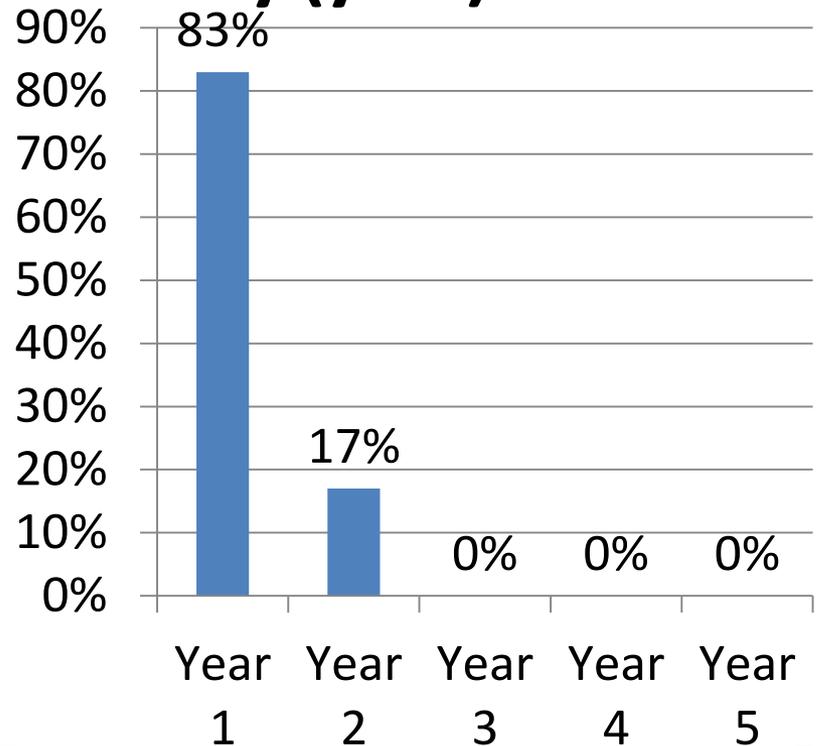
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# Wing-Span vs. Weight-Based Fees/ Rates & Charges

## Select Timeframe Priority (year)

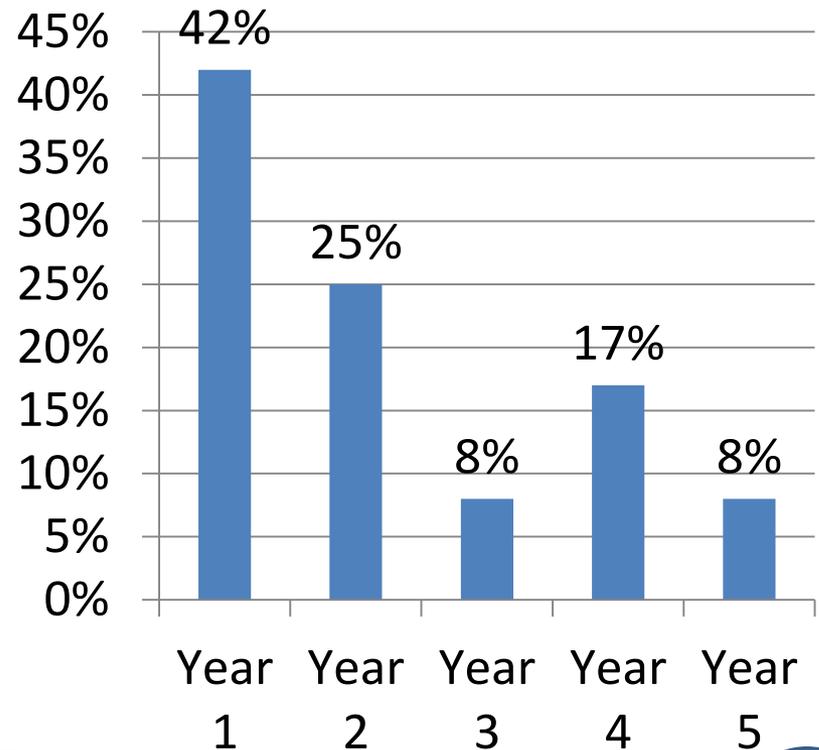
1. Year 1
2. Year 2
3. Year 3
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5. Year 5



# Combo GA Hangars/Commercial Space

## Select Timeframe Priority (year)

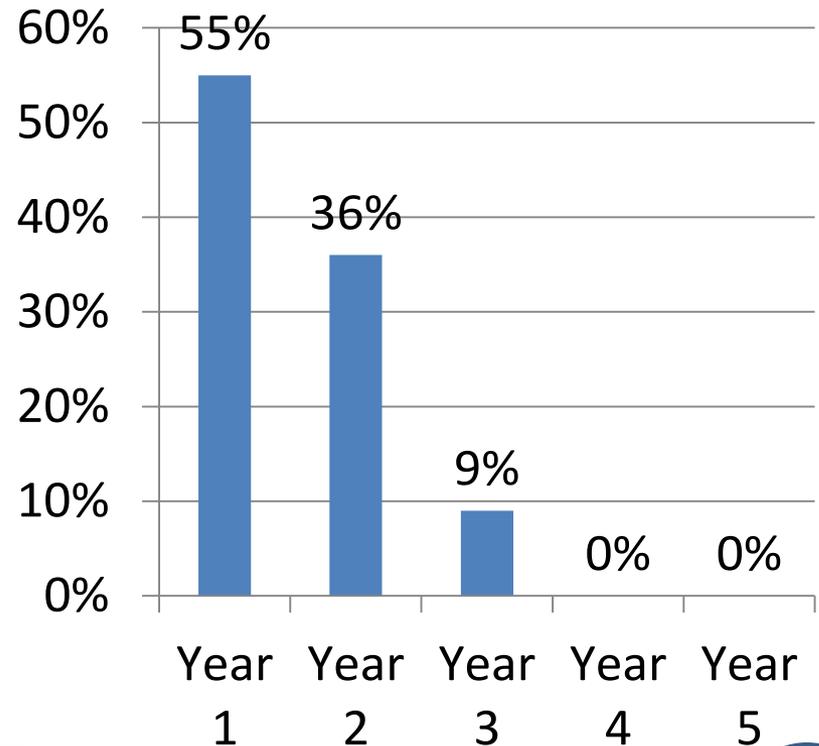
1. Year 1
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# Expand Bunker Area Industrial Development

## Select Timeframe Priority (year)

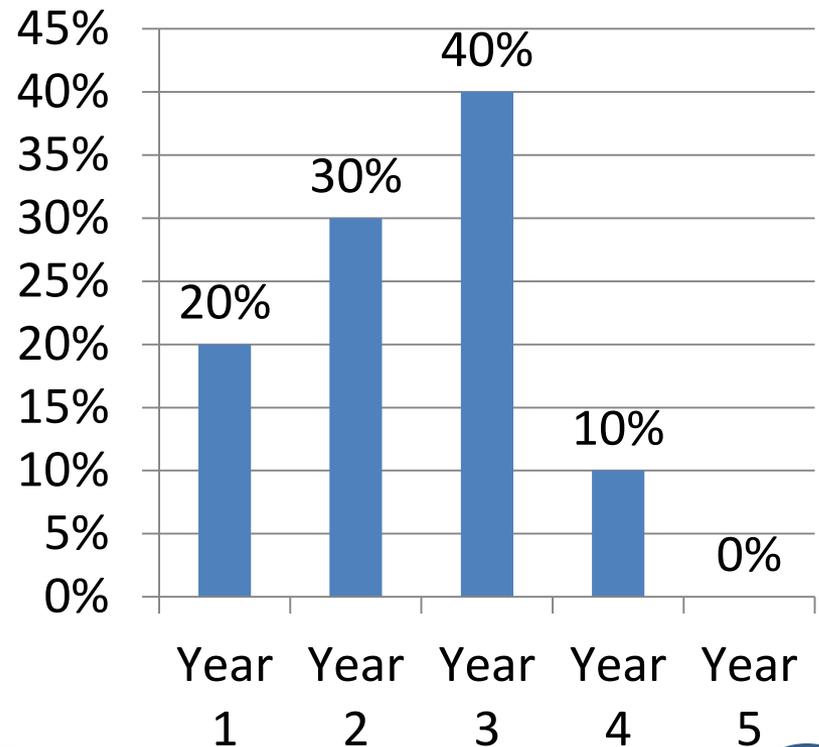
1. Year 1
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3. Year 3
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# Delta Parcel Lease

## Select Timeframe Priority (year)

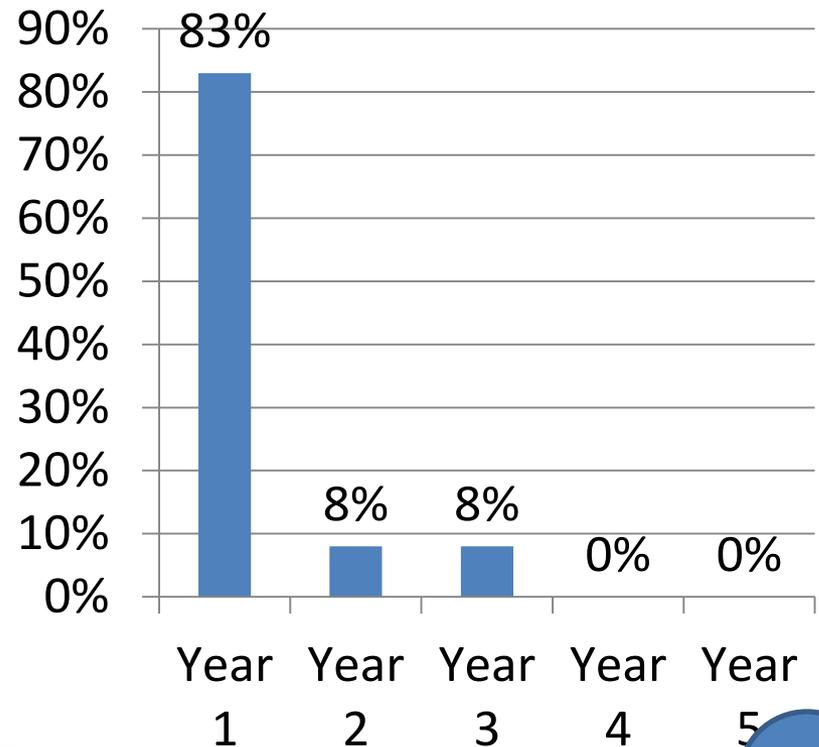
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# Parking Lot Operations & Rates

## Select Timeframe Priority (year)

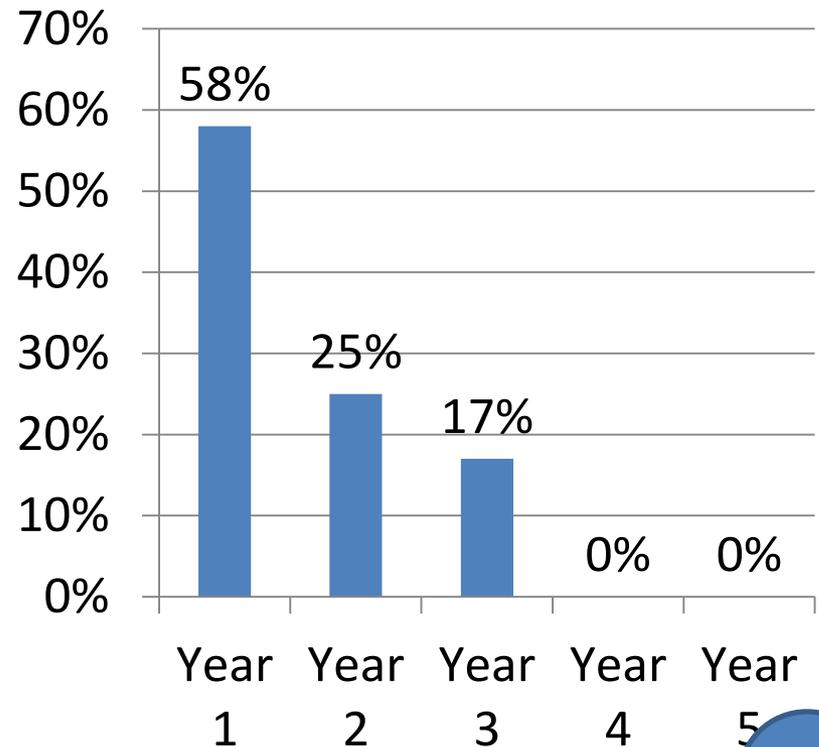
1. Year 1
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# Bunker Road Commercial Vehicle Parking

## Select Timeframe Priority (year)

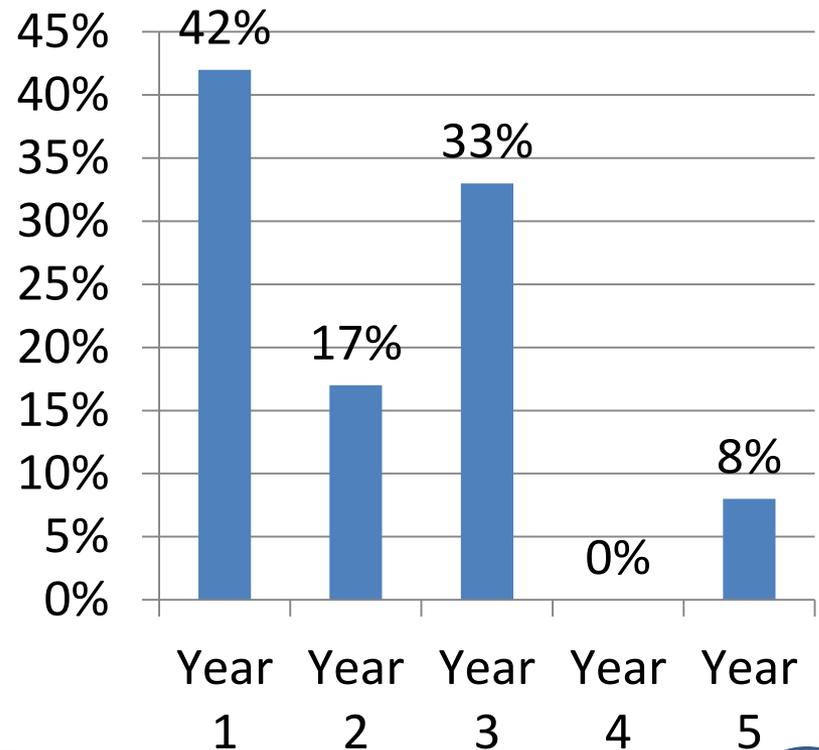
1. Year 1
2. Year 2
3. Year 3
4. Year 4
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# Flex Space Use: Terminal and GA Bldg.

## Select Timeframe Priority (year)

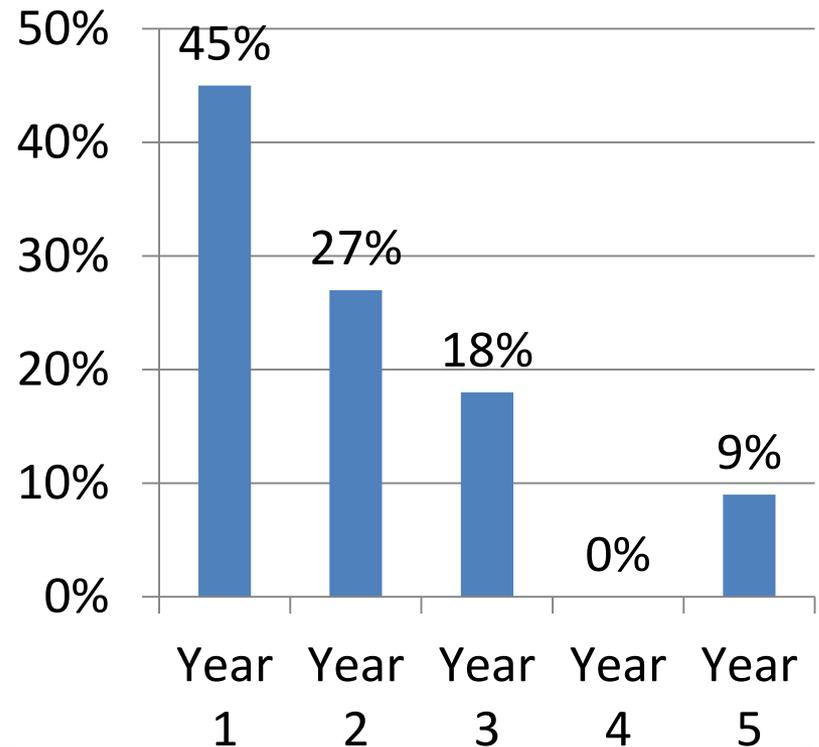
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# GA Revitalization & Special Events

## Select Timeframe Priority (year)

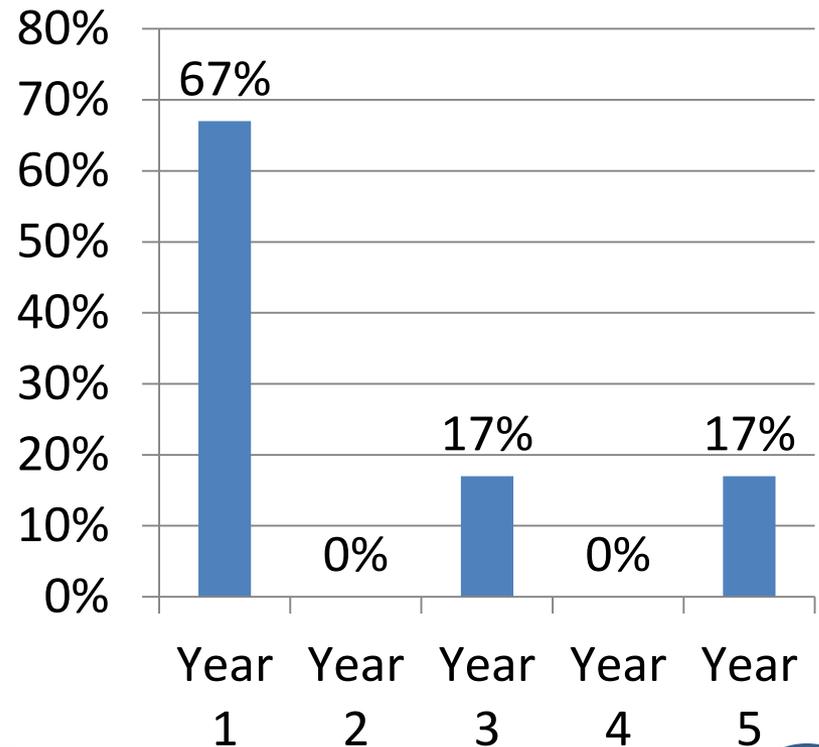
1. Year 1
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# Coastal Management Initiative

## Select Timeframe Priority (year)

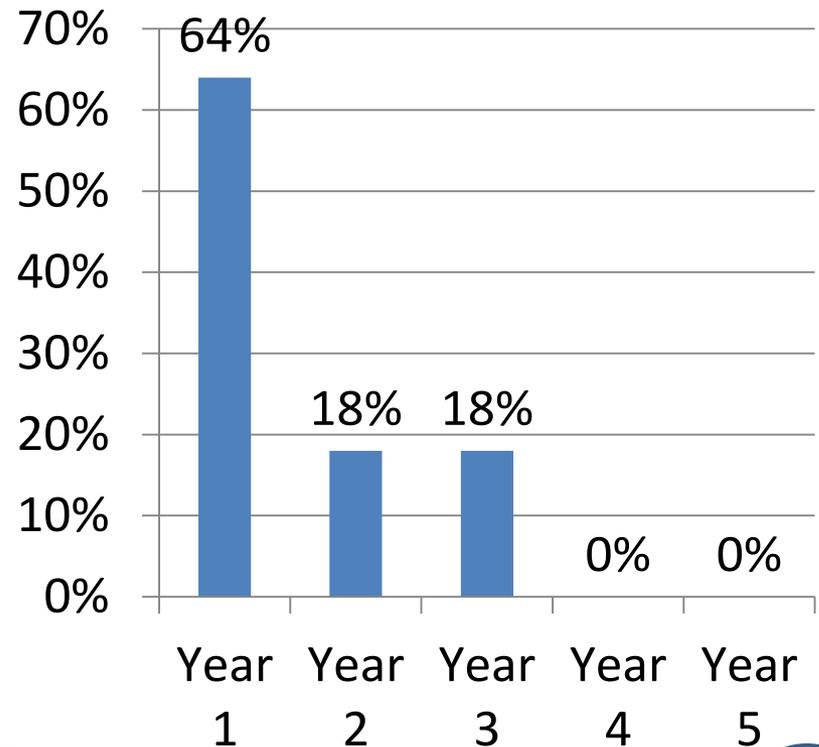
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3. Year 3
4. Year 4
5. Year 5



# Apron Lighting Control

## Select Timeframe Priority (year)

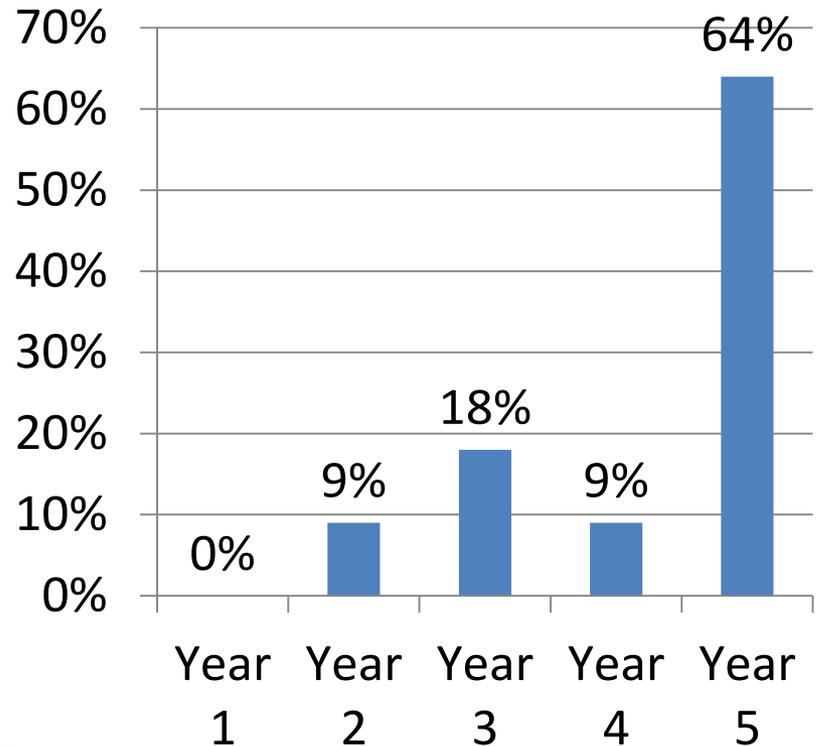
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3. Year 3
4. Year 4
5. Year 5



# Ramp Electrification—VALE (Voluntary Airport Low Emissions Program)

## Select Timeframe Priority (year)

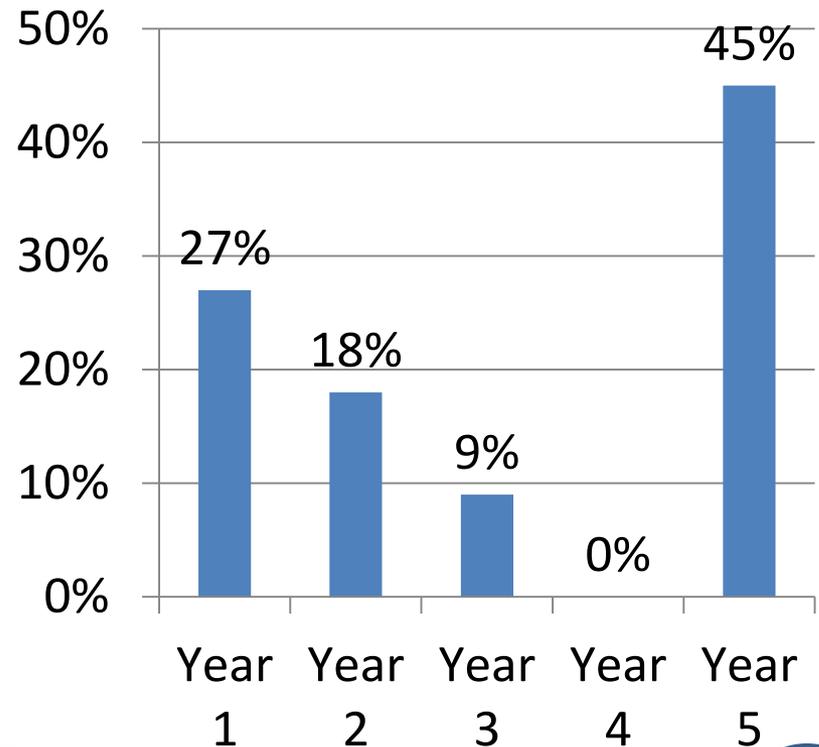
1. Year 1
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# Bunker Area Solar Development

## Select Timeframe Priority (year)

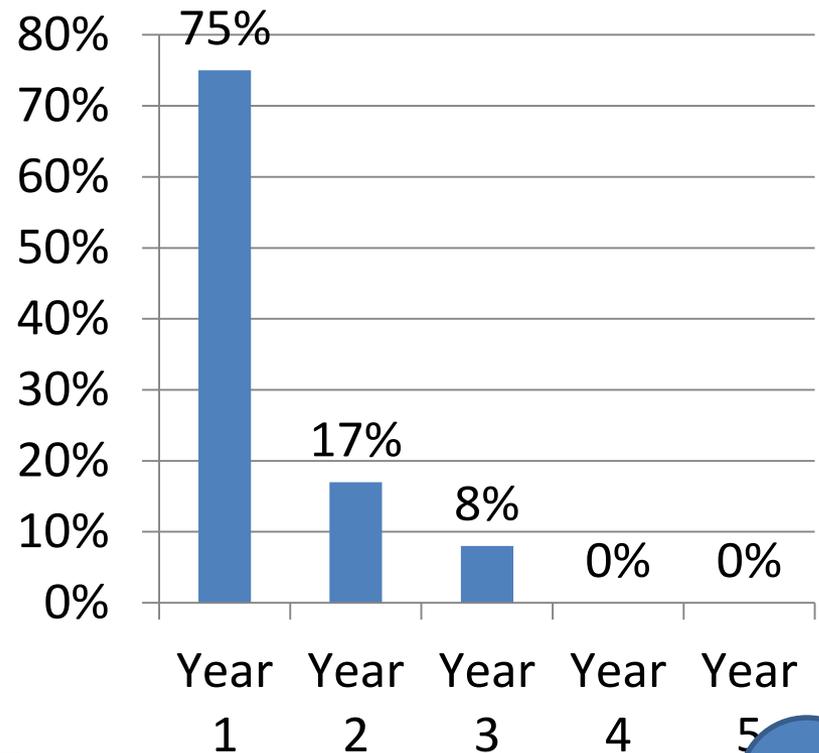
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3. Year 3
4. Year 4
5. Year 5



# “Fly Friendly” Aircraft Noise Mitigation

## Select Timeframe Priority (year)

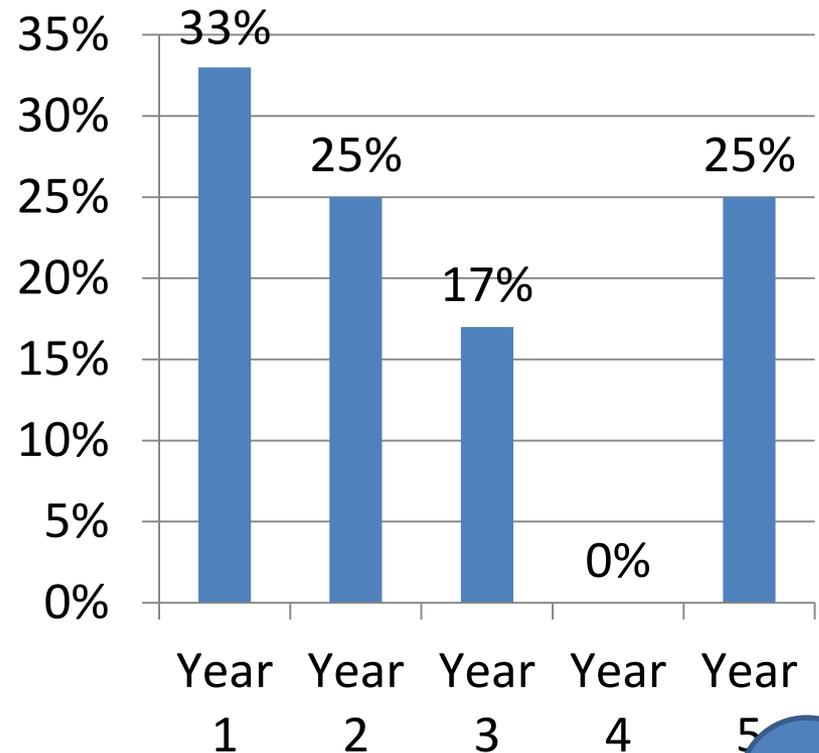
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3. Year 3
4. Year 4
5. Year 5



# Promote Bike, Transit, Vans, etc,

## Select Timeframe Priority (year)

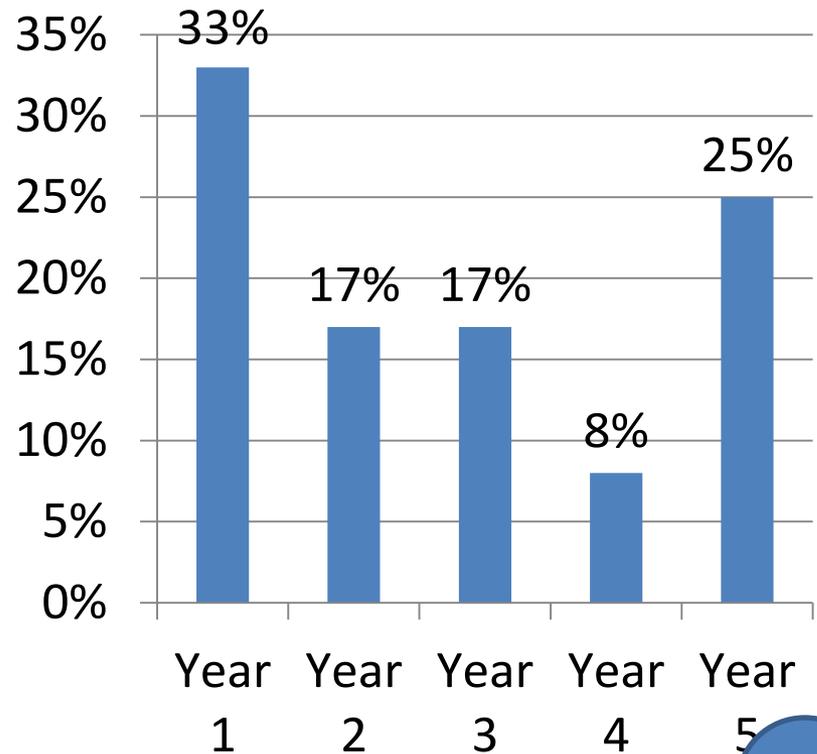
1. Year 1
2. Year 2
3. Year 3
4. Year 4
5. Year 5



# Bike Share/Rental Program

## Select Timeframe Priority (year)

1. Year 1
2. Year 2
3. Year 3
4. Year 4
5. Year 5



Nantucket Airport Master Plan  
 MASTER PLAN ALTERNATIVE CONCEPTS - PRIORITY LIST

**Advisory Group Preference Ratings**

0 Years- 5 Years

Project Chapter	Project	Engineers Probable Cost	FAA Eligible		Recommended		Total Time Frame 0 Years - 5 Years	Time Frame				
			Yes	No	Yes	No		1	2	3	4	5
<b>CAPACITY/TERMINAL AIRFIELD CONCEPTS: 0-5 Years</b>												
7.2.1	Terminal Secure Hold Room Concepts											
	Seasonal Tent/Secure Hold Room	\$20K		✓			X	60%	20%	20%	0%	0%
<b>Sub Total=</b>		<b>\$20K</b>										
<b>EFFICIENCY-ACCESSORY CONCEPTS: 0-5 Years</b>												
7.3.4	Need for Providing Airport Manager's House	\$750K		✓			X	64%	18%	18%	0%	0%
	Air Traffic Control Tower Rehabilitation	\$1M		✓			X	64%	18%	9%	0%	9%
<b>Sub Total=</b>		<b>\$1.75M</b>										
<b>REVENUE-ENHANCEMENTS: 0-5 Years</b>												
7.4.10	Wingspan vs. Weight-based Fees/Rates and Charges	N/A		✓	✓		X	83%	17%	0%	0%	0%
7.4.2	Combo GA Hangars/Commercial Space (\$2.5M Private Costs)	N/A		✓	✓		X	42%	25%	8%	17%	8%
7.4.6	Expand Bunker Area Industrial Development	N/A		✓	✓		X	55%	36%	9%	0%	0%
7.4.7	DELTA Parcel/Public Employee Housing/Seasonal Employee	N/A		✓	✓		X	20%	30%	40%	10%	0%
7.4.4	Parking Lot Operations: Hourly vs. Overnight Rates / Access Gate	\$80K		✓	✓		X	83%	8%	8%	0%	0%
7.4.5	Bunker Road Commercial Vehicle Parking Areas	\$15K		✓	✓		X	58%	25%	17%	0%	0%
7.4.11	Revenue and Enhancement Concepts - Flex Space Terminal/GA	N/A		✓	✓		X	42%	17%	33%	0%	8%
7.4.12	GA Revitalization/Special Events/Owner Type Group Fly-ins	N/A		✓	✓		X	45%	27%	18%	0%	9%
<b>Sub Total=</b>		<b>\$2.6M</b>										
<b>ENVIRONMENTAL-SUSTAINABILITY: 0-5 Years</b>												
7.5.3	Coastal Management Initiative (On-Going)	\$250K (\$50K Per-Year for 5 Years)		✓	✓		X	67%	0%	17%	0%	17%
7.5.12	Apron Lighting Control/PCL Dimmer Concept	\$80K	✓		✓		X	64%	18%	18%	0%	0%
7.5.11	Ramp Electrification- VALE	\$4M		✓	✓		X	0%	9%	18%	9%	64%
7.5.1	Solar Array Development											
7.5.1.1	Bunker Area Solar Development	N/A		✓	✓		X	27%	18%	9%	0%	45%
7.5.10	"Fly Friendly" Aircraft Noise Mitigation Measures (On-Going)	\$50K (\$10K (Per-Year)		✓	✓		X	75%	17%	8%	0%	0%
7.5.6	Advertise Rental Cars/Taxis/Bike/Courtesy Vans/NRTA	\$25K (\$5K Per-Year)		✓	✓		X	33%	25%	17%	0%	25%
7.5.8	Bike Share/Rental Program	N/A		✓	✓		X	33%	17%	17%	8%	25%
<b>Sub Total=</b>		<b>\$4.4M</b>										

AIP ELIGIBLE